

## T – MARKING PROCEDURES

### INTRODUCTION

T-marking is the placement of a paint marking on the pavement using the No Passing Zone Logs prepared in the office. Using the DMI, distance is measured from reference points and the road is marked according to the No Passing Zone Log with symbols shown in Table 5.

### REQUIRED ITEMS

1. Distance Measuring Instrument (DMI).
2. No Passing Zone Logs.
3. Orange Spray Pain – at lease on case.
4. Black Spray Paint – for covering mistakes.
5. Rotating Light for Vehicle.
6. Orange Flag.
7. Wheel.
8. Miscellaneous Supplies: Note pad on clip board, pencils, sharpener, stapler, paper clips, post-it-notes, file folders, etc.

### DUTIES OF THE FIELD PERSONNEL

#### Marker

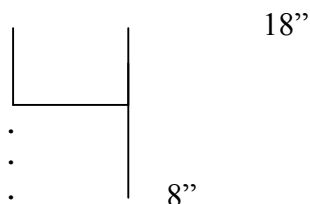
Paints the symbols in the center of the road according to the No Passing Zone Log.

#### Flagger

Stops traffic if necessary, and makes sure the marker is not in any danger from approaching vehicles.

### THE MARKING SYSTEM

The size of the T-marking symbols are approximately 18”8”



The symbols need to be large enough to be seen by the driver of the paint truck traveling at low speeds.

**TABLE 5**  
**SYMBOLS**

## REFERENCE

Indiana Manual on Uniform Traffic Control Devices for Streets and Highways, 1988, Indiana Department of Highways, Part III Markings.

National Manual on Uniform Traffic Control Devices for Streets and Highway, 1988, US Department of Transportation Federal Highway Administration, Part III Markings.

Roadstar Distance Measuring Instruments Operating Manual, 1987, Nu-Metrics Inc., Section Three (Calibration) and Section Seven (Operating Procedures).

June 29, 1993

County Engineer  
1 West Superior Street  
Fort Wayne, IN 46802-1299

Dear Sir:

This is to certify that all No Passing Zones included in the No Passing Zone Log for Allen County, Indiana dated June 29, 1993 were determined in accordance with the Indiana Manual on Uniform Traffic Control Devices, dated 1988, and the National Manual on Uniform Traffic Control Devices dated 1988.

---

Steven R. Hardesty, P.E

## GENERAL NOTES

Where sidewalk is constructed immediately adjacent to curb, the surface of the sidewalk shall be constructed in higher than the top of the curb.

Transverse joints shall be cut with a jointer having a radius of 1" inch of spacing as indicated or directed.

\*Compacted Subgrade – All soft, yielding or other unsuitable material, which cannot be compacted satisfactorily, shall be removed. All rock encountered shall be removed or broken off at least six (6) inches below the subgrade surface. Any holes or depressions resulting from the removal of unsuitable material shall be filled with satisfactory material and compacted to conform to the surrounding subgrade surface.

\*\*Where sidewalk is constructed adjacent to curb, the space behind the new curb shall be filled with granular material to the required elevation and compacted in layers not to exceed 4".

\*\*\*Where sidewalk is not constructed adjacent to the curb, the space behind the curb shall be filled with suitable material to the required elevation and compacted in layers not to exceed 4".

\*\*\*\*When built in conjunction with concrete pavement, contraction joints should be placed at the same locations as in the pavement slab. The curb and gutter should be tied to the pavement by ½" round deformed bars at about 3 foot intervals. If no concrete pavement is being built at the time the curb is constructed, expansion joints should be placed at the ends of all returns and of intervals not to exceed in 100 feet. Contraction joints should be installed at 20 foot spacing.